

I-59/20 CENTRAL BUSINESS DISTRICT (CBD) INTERCHANGE AND BRIDGE REPLACEMENT PROJECT

**American Society of Civil Engineers
March 02, 2017**



**DeJarvis Leonard
East Central Region Engineer
Alabama Department of Transportation**

Decision still months away on freeway routes through downtown areas and outlying districts

Public to be given chance to speak mind

By Irving Kriman
News Staff Writer

A time for decision in the selection of east-west and north-south freeway routes through Birmingham is still months away.

U. S. Justice State Highway Division engineers in charge of the area federal highway re-licensing covers for Alabama, made that clear Monday.

The reason for the delay, with mention of a committee headed by C. T. Jones Jr., Birmingham attorney, which is studying a proposed 200-ft route for the proposed north-south freeway through the Birmingham district.

JORDAN'S TALK developed the information, issued after three days of the two routes.

The two freeways will cut around 1000 acres. There is no breakdown as to the amount of the cost of each.

A tentative east-west route has been chosen.

HOWEVER, A "PUBLIC" hearing will be held in a few weeks to let interested parties can air their views on the route, and so that decisions will be made for the State Highway Department officials that hearing.

State Dept. the department's proposed route will be approved by the U. S. Bureau of Public Roads.

3. The tentative north-south route is 200 feet from the center line to the center line. The route will follow the center line and will have four lanes in each direction. The route will be 200 feet from the center line to the center line. The route will be 200 feet from the center line to the center line.

4. Jordan said the 200-ft route has been approved in the Birmingham district and will be completed within six months. The route will be 200 feet from the center line to the center line.

HE SAID THE proposed 200-ft route will be 200 feet from the center line to the center line. The route will be 200 feet from the center line to the center line.

Conservative members said they would be a 100-ft wide "control area" as possible about the proposed route as that highway would have something about the proposed highway.

The question was asked, "Will the city have retained a right-of-way along the State Highway Department outside a tentative route as the north-south freeway approach?"

JORDAN SAID, "No, there will be 100-ft wide local authorities and private lands, and a local public hearing also will be held."

Jordan said traffic studies by State Highway Dept. around the route would be done in the next few months of approval.

He said a study several years ago showed that 30 per cent of the traffic crossing Red Mountain goes into the general downtown area.

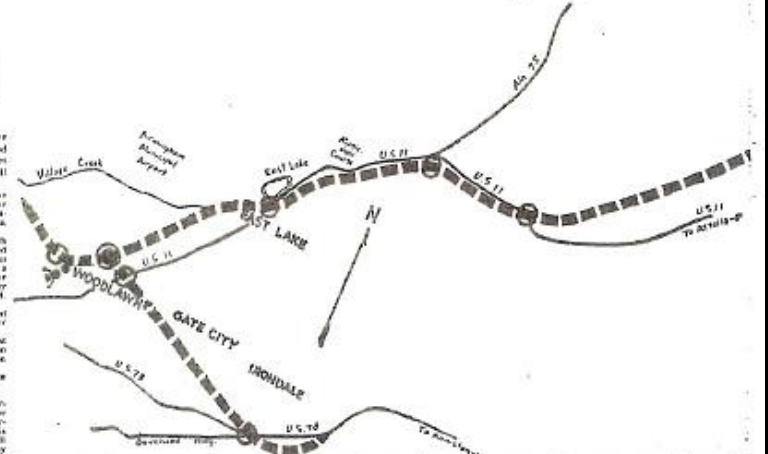
He added that recent studies show that more than 50 per cent of all the traffic leaving the metropolitan Birmingham area will stop in Birmingham.

APPARENTLY, it was the freeway will go through downtown Birmingham at some time, rather than into the general downtown business district.

Like members of the committee opposing the 200-ft route asked Mr. Jordan, "If you change your route from the freeway into the center of town, what are you going to do with it? The traffic situation is already hopeless."

JORDAN SAID the highway Department will also have a detailed plan for the north-south artery.

He also pointed out that the department is thinking in terms of the possibility of building a toll to help what will serve the peak hour when traffic conditions are envisioned for that period.



Eastern portion of east-west route—This map shows the tentative location for the eastern portion of the new east-west highway which would pass through the Birmingham district. Circles along the proposed routes in all three diagrams are "approved control areas."



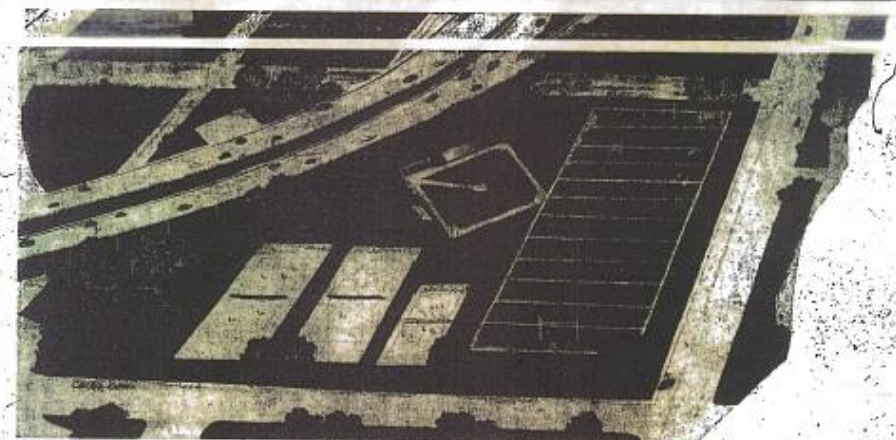
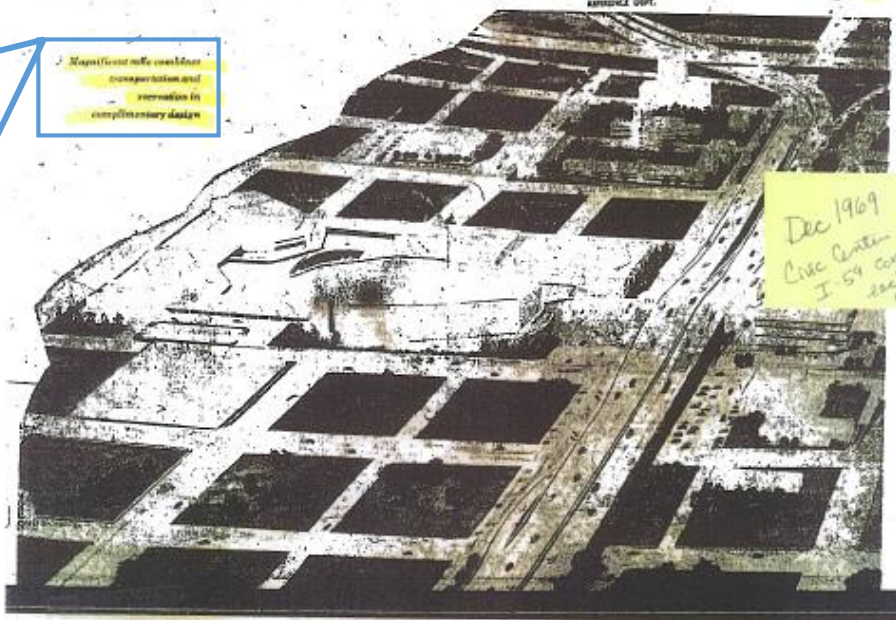
Central portion of tentative east-west freeway route—This map continues the tentative route for the proposed east-west highway through the Birmingham district. The final route decided upon will not be made known until a legal public hearing is held several months from now.



PLANNING ARTICLE

from

March 1959



Vision materializes from red mud, rubble

BY FRANCES SPENCER, News and writer
... vision for the '60s in remodeling and red mud and rubble
... of Birmingham's new highway system
... of Birmingham's new highway system
... of Birmingham's new highway system

Magnificent mile combines transportation and recreation in complimentary design

Magnificent mile combines transportation and recreation in complimentary design

MIRACLE MILE ARTICLE from 12/29/1969

I-59/20 & I-65 INTERCHANGE SOUTH VIEW



EXISTING

I-59/20 & I-65 INTERCHANGE SOUTH VIEW



PROPOSED

I-59/20 & RME INTERCHANGE EAST VIEW



EXISTING

I-59/20 & RME INTERCHANGE EAST VIEW



PROPOSED

I-59/20 DOWNTOWN SHERATON



EXISTING

I-59/20 DOWNTOWN SHERATON



PROPOSED

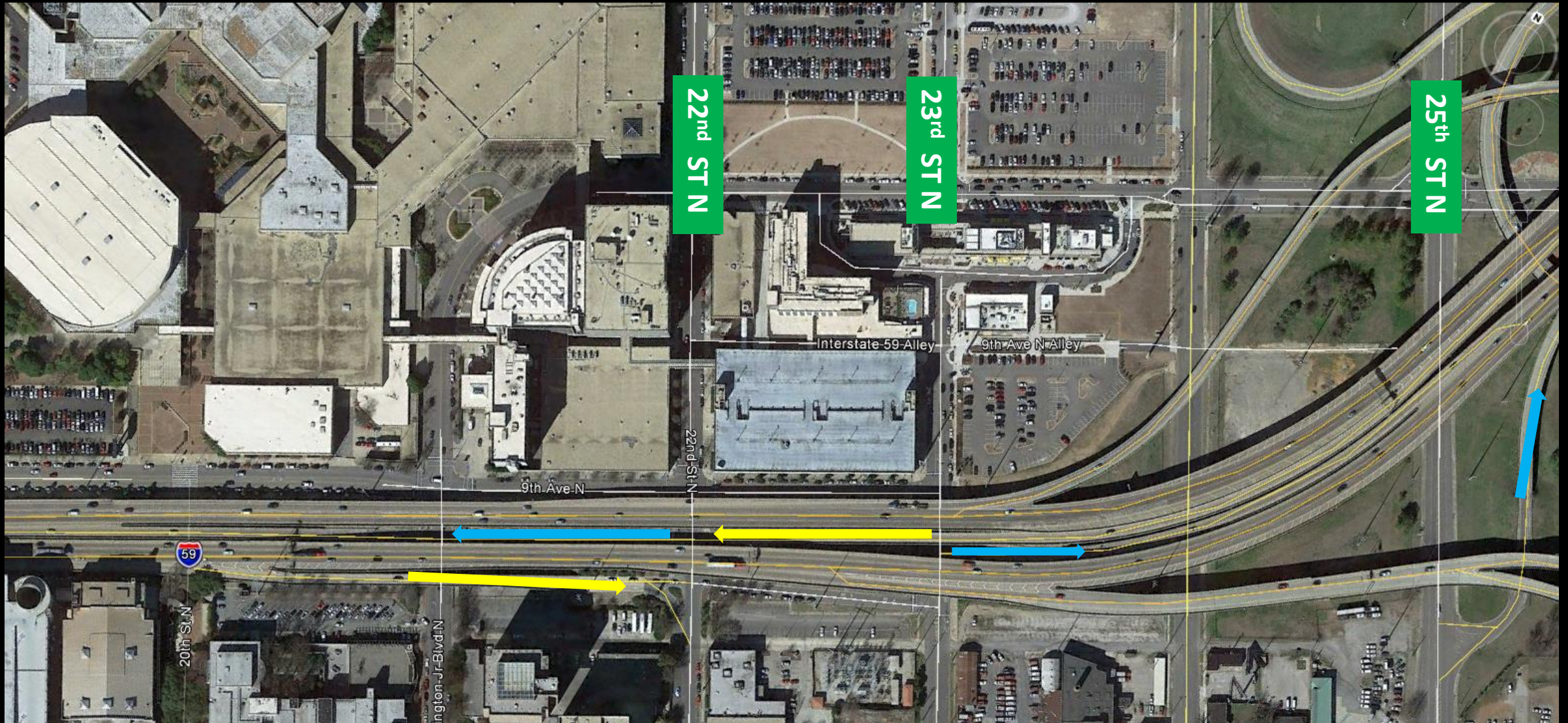
CURRENT CONDITIONS/ISSUES



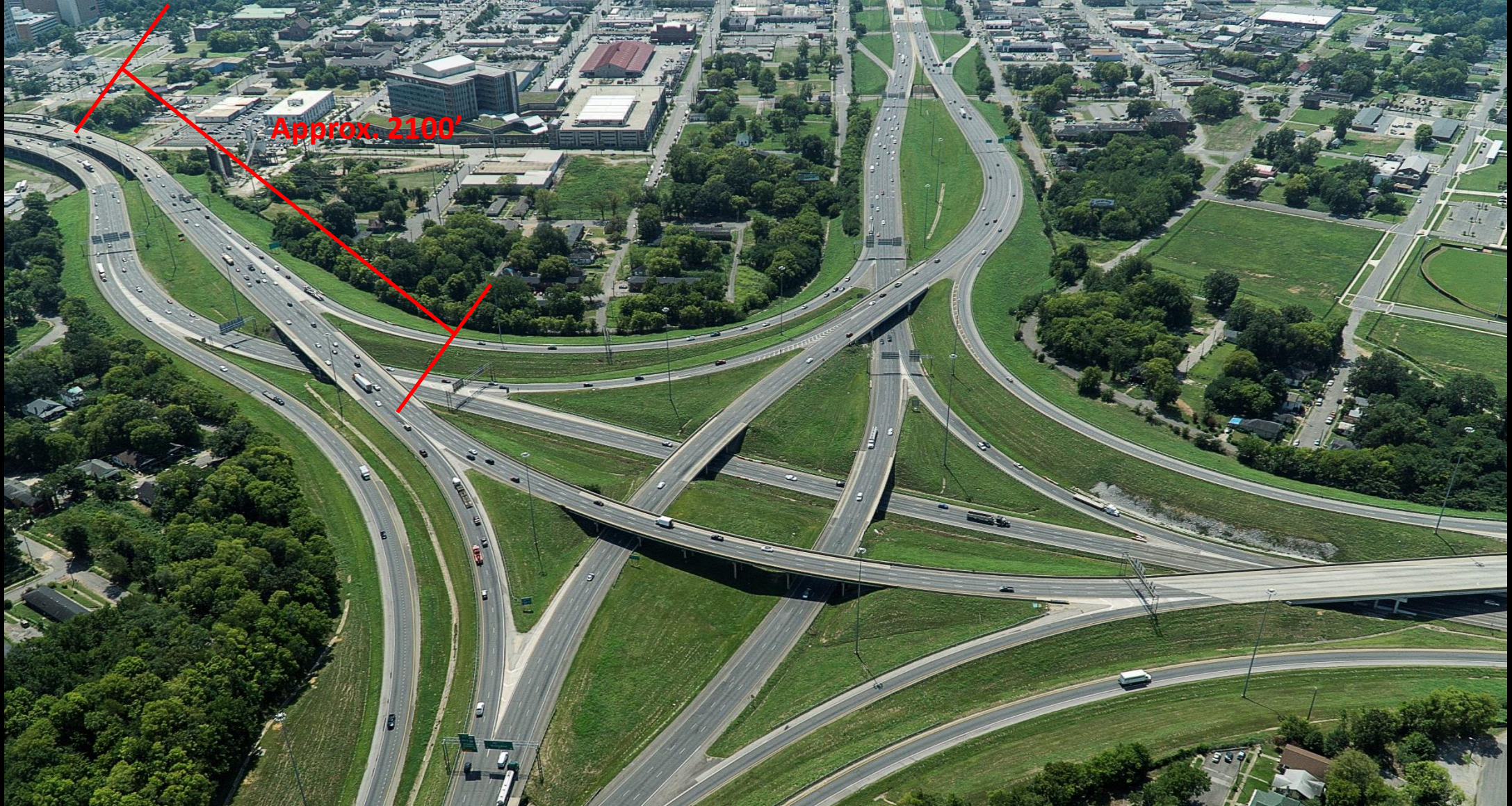
CURRENT CONDITIONS/ISSUES



CURRENT CONDITIONS/ISSUES



CURRENT CONDITIONS/ISSUES



A BETTER WAY



A BETTER WAY



CURRENT CONDITIONS/ISSUES



A BETTER WAY



CURRENT CONDITIONS/ISSUES



A BETTER WAY



CURRENT CONDITIONS/ISSUES



A BETTER WAY



CURRENT CONDITIONS/ISSUES



A BETTER WAY



PHASES OF CONSTRUCTION



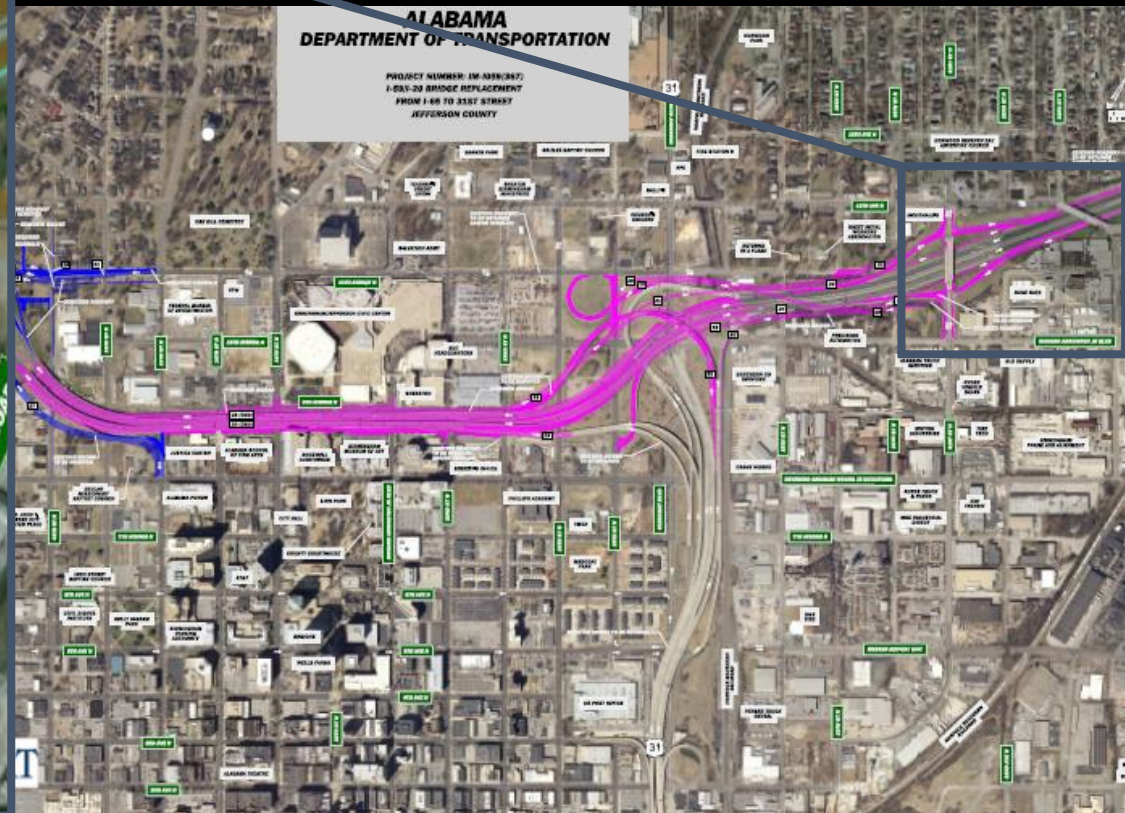
BRIDGE DESIGN PROJECT SCOPE

- Phase A - 0.13 Mile of Bridges
- Phase 1 – 0.19 Mile of Bridges (Widening & Jacking)
- Phase 2 – 3.73 Miles of Bridges
- Phase 3 – 2.48 Miles of Bridges (Does not Include Segmental)

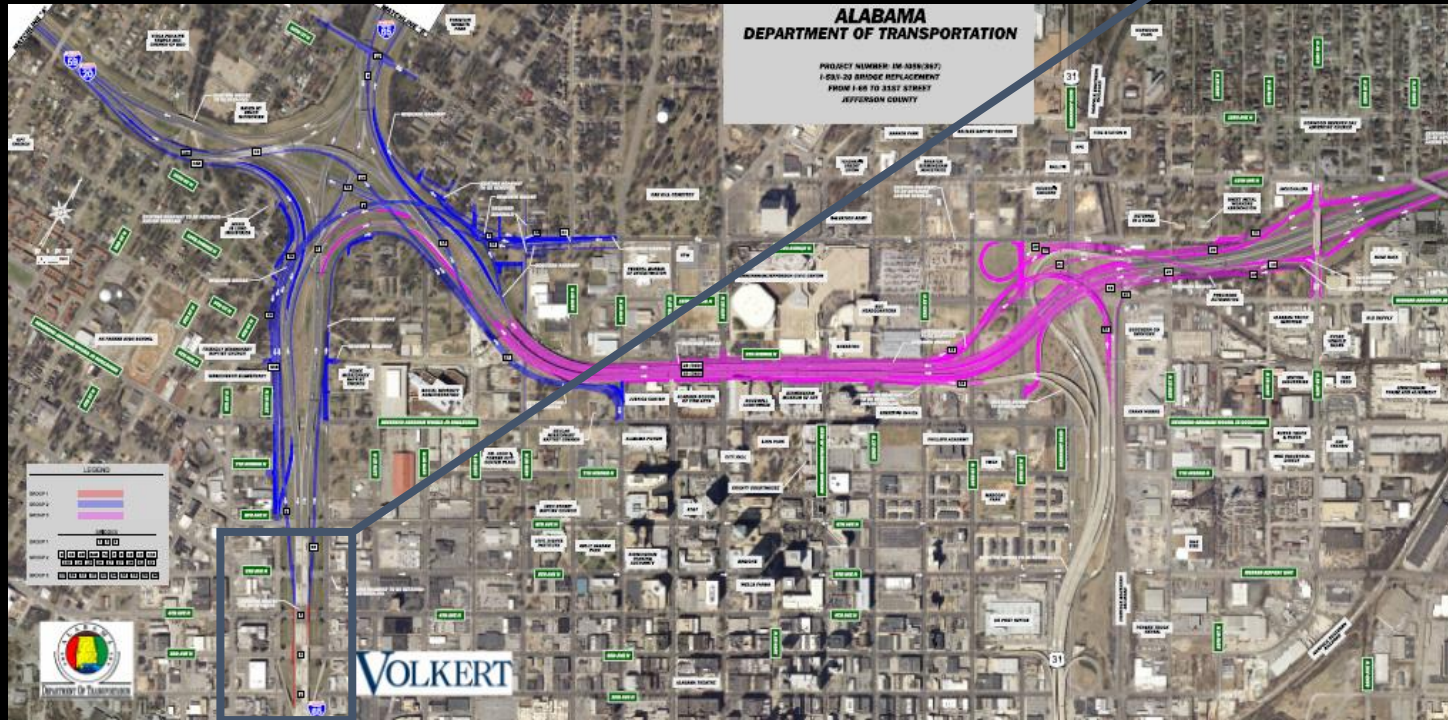
Total Support Bridges Phases A, 1, 2, 3 – 6.53 Miles

Phase A

\$19,281,545







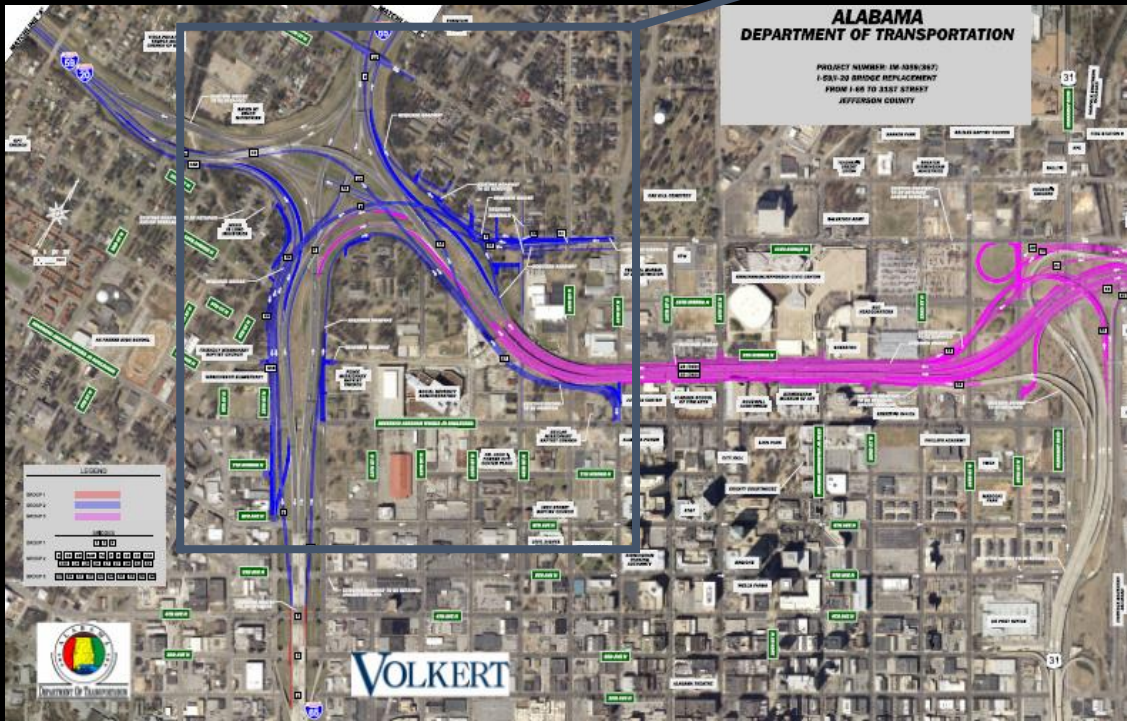
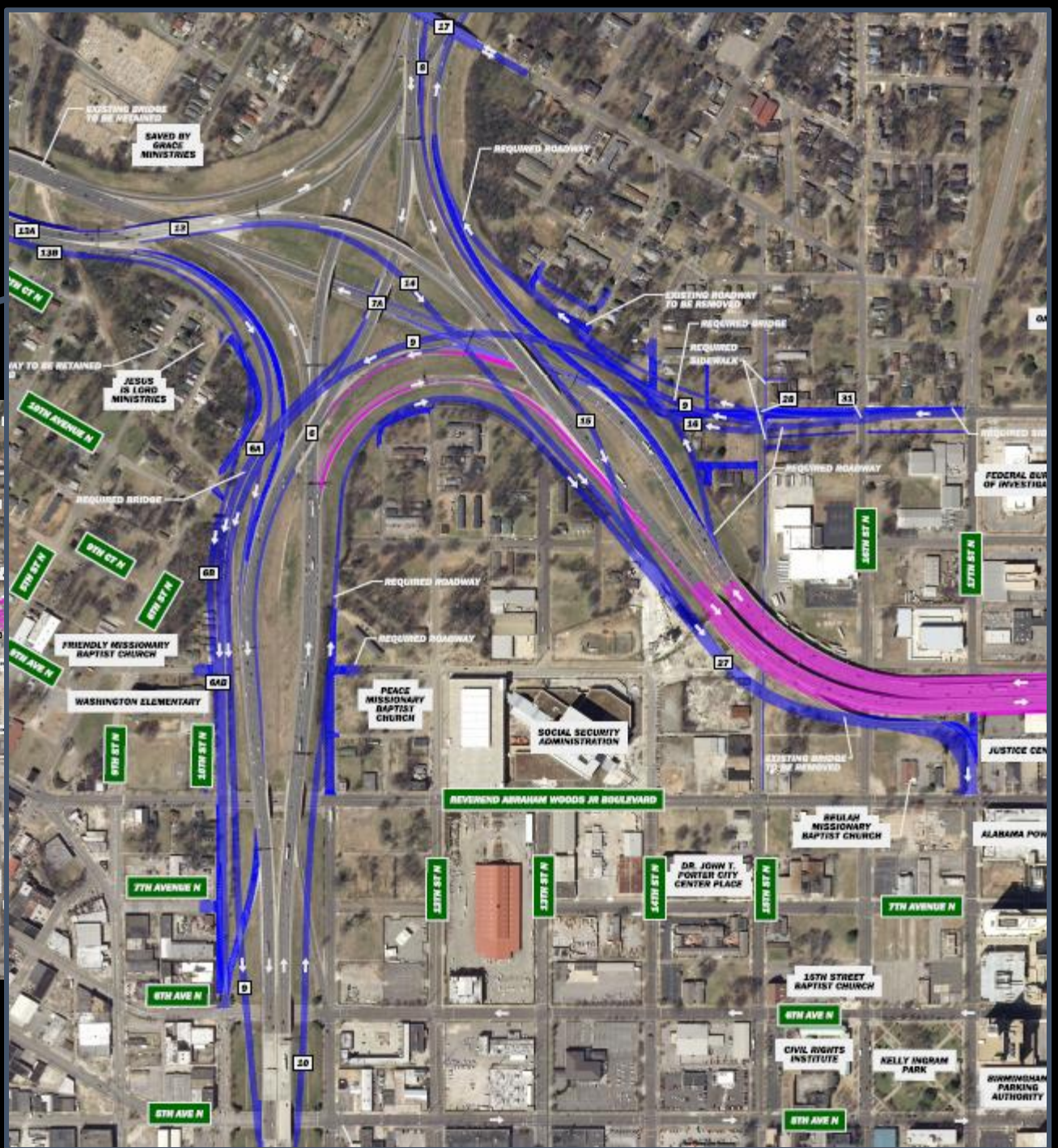
Phase 1
\$7,416,165





Phase 2

\$208,611,848









FOUNDATION INSTALLATION



H-PILES



DRILLED SHAFTS



DRILLED SHAFTS



GEOPIERS



GEOPIERS



MICRO-PILES



MICRO-PILES



MICRO-PILES



MICRO-PILES



MICRO-PILES



MICRO-PILES



RETAINING WALLS (MSE)



RETAINING WALLS (MSE)



RETAINING WALLS (SOLDIER PILE)



RETAINING WALLS (SOLDIER PILE)



RETAINING WALLS (SOLDIER PILE)



RETAINING WALLS (SOLDIER PILE)



RETAINING WALLS (SOLDIER PILE)



RETAINING WALLS (SOLDIER PILE)



RETAINING WALLS (SOLDIER PILE)



RETAINING WALLS (SOLDIER PILE)



RETAINING WALLS (SOLDIER PILE)



RETAINING WALLS (SOLDIER PILE)



RETAINING WALLS

- Sculpted Shotcrete Pattern Facing for Soldier Pile Wall



RETAINING WALLS (SOIL NAIL)



GIRDER INSTALLATION



GIRDER INSTALLATION



GIRDER INSTALLATION



GIRDER INSTALLATION



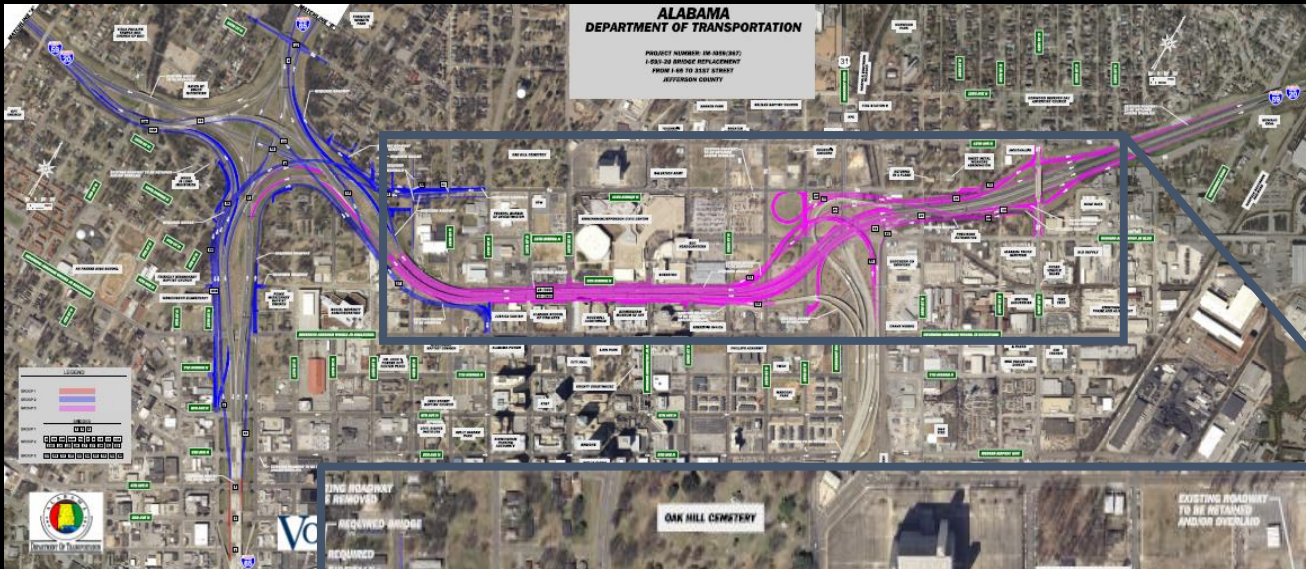
GIRDER INSTALLATION



INCENTIVE FOR PHASE 2

- IF COMPLETED ON OR BEFORE AUGUST 31, 2018 –
CONTRACTOR WILL RECEIVE A **\$10 MILLION DOLLAR BONUS**
- IF NOT COMPLETED BY AUGUST 31, 2018 –
CONTRACTOR WILL BE ASSESSED A DISINCENTIVE OF **\$200,000**
PER DAY





Phase 3







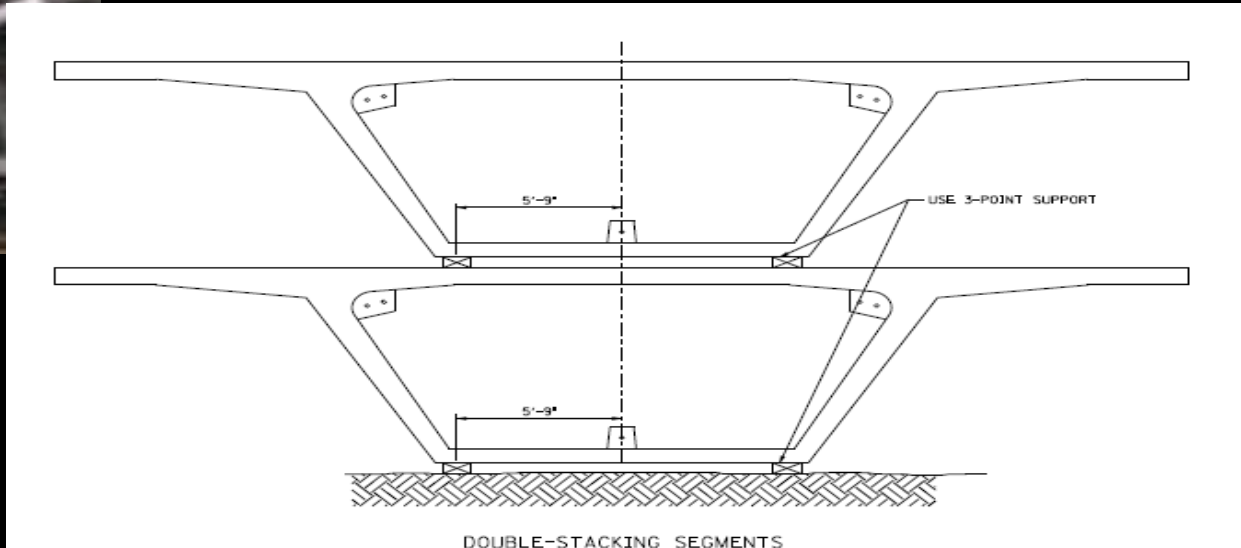
SEGMENTAL BRIDGE SECTION

EXAMPLE – PHASE 3



SEGMENT STORAGE

EXAMPLE – PHASE 3



ERECTION TRUSS / SHORING TOWER

EXAMPLE – PHASE 3



OVERHEAD GANTRY EXAMPLE – PHASE 3



ESTIMATED PROJECT TIMELINE

*Estimated closures are based on the contractor's preliminary schedule and are subject to change

<u>Phase</u>	<u>Project</u>	<u>Duration</u>
A	Reconstruction of 12 th Avenue N & 31 st Street N Bridges	May 2015 – June 2017
1	Bridge Widening on I-65 over 2 nd , 3 rd and 4 th Avenue N	August 2015 – June 2016
2	Reconstruction of I-65 and I-59/20 Interchange Ramps	March 2016 – August 2018
3	Reconstruction of RME Ramps & Reconstruction of Mainline Bridges	Summer 2017 – Fall 2020
3 (Milestone)	Closing of I-59/20 Bridges	TBD

PHASE 3

ALTERNATIVE TECHNICAL CONCEPTS (ATC's)

1. Time Saving
2. Construction Sequencing
3. Construction Methods
4. Less Traffic Impacts



PHASE 3

CONTRACTOR'S PROPOSAL

Base Proposal	ATC Proposal
- Unit Pricing	- Lump Sum Pricing



PHASE 3

MAINLINE BRIDGE RECONSTRUCTION TIMELINE

Single Lane Closure	Double Lane Closure	Complete Closure
\$42,500/day per direction	\$105,000/day per direction	\$125,000/day per direction

- Contractor proposes number of days required for each type of closure
- This method will be used to determine any incentive or disincentives based on the contractor's proposed schedule vs. actual completion time



See Map on Back



Please Use Caution/ Bridge

The 12th Court North Bridge must be closed begin

Who

Parents and others driving to Elementary and Wilkerson M alternative routes, adjust arr



ALABAMA DEPARTMENT OF TRANSPORTATION
East Central Region
100 Corporate Parkway, Suite 450
Hoover, AL 35242

Road Closure Alert!

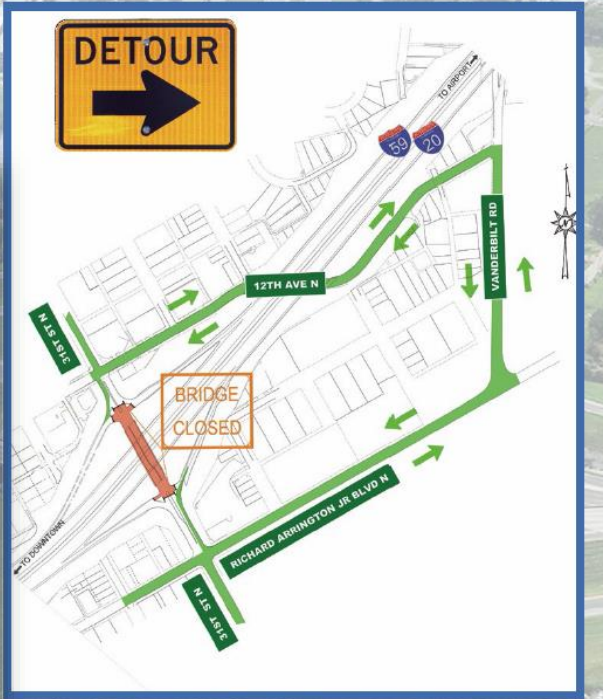
Closure Alert!

31st Bridge to Close.

On October 6th, the 31st Street bridge over I-59/20

PRST STD
U.S. POSTAGE
PAID
BIRMINGHAM AL
PERMIT NO. 585

ADDRESS HERE



Improving Birmingham's Interstate Assets

Stay in touch with the progress, and if you have questions you can contact us at:

On the web, go to:
www.5920bridge.com

Email us at:
contact@5920bridge.com

Facebook.com/5920bridge
@5920bridge

improvements for Alabama. Transportation





Improving Birmingham's Interstate Assets

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5920bridge.com

When the Interstate System was first developed in the 1940s, the federal government specified that interstate highways weave through all American cities with populations greater than 100,000 people, at locations that could be easily accessed by a majority of the population. In practice, this meant the interstate was often run through the center of town, or alongside heavily populated residential areas. Because of age, the Alabama Department of Transportation has begun an effort to improve and replace the elevated portions of Interstate 59/20 through downtown Birmingham. Information on the scope of this project can be found at this site. In addition, it will be periodically updated to keep you informed of our progress.

NEWS

31st Street Bridge over I-59/20 Closure

I-59S and I-20W at 17th street Closure

I-65 NB 16TH Street Exit Ramp Closing

UPDATES

October 4th Monthly Briefing

Phase One work continues on I-65N near Ex. 261-A



@5920bridge



@5920bridge

JOIN US ON OUR LIVE MONTHLY UPDATES

Media Briefing

December 21, 2016

2:00 PM

DeJarvis Leonard

East Central Region Engineer

****Message from Organizer****

Welcome to the 59/20 Bridge Media Briefing. During the presentation, all microphones will be muted. If you have any questions during the presentation, please use the chat feature. At the end of the presentation, each attendee will be unmuted individually and given the opportunity to ask questions.

****To improve audio quality, please call into the conference by phone.****

Please call into the conference using the instructions in the audio control panel.



Linda Crockett, ALDOT
Public Information Officer

205-327-4973

crockettl@dot.state.al.us

BENEATH THE BRIDGE CONCEPT



LIGHTING CONCEPT



Q&A SESSION



www.5920bridge.com